

## SECTION 4.0 DEVELOPMENT STANDARDS

The Specific Plan Area has been broadly divided into three major districts, the High Density Residential District, the Station Plaza District, and the Mixed Use District. The High Density Residential District forms the western edge of the site adjacent to the existing single family residential neighborhood. The Station Plaza District is the area immediately to the north of the proposed station and forms the entry to the Specific Plan Area from the platform. The remainder of the site is designated as a Mixed Use District, which allows for the flexibility for the site to adapt to changing market conditions over a longer period of time.

This Section describes all the standards and guidelines for street design, site planning, and building design for the Specific Plan Area. The regulations are district and building specific. These are the regulations that govern new construction, as well as alterations and additions, in the Specific Plan Area.

To create a vibrant, thriving and special community, the Development Standards for the Specific Plan Area are “Form Based” to create a predictable public realm by establishing guidelines and regulations that focus primarily on the physical form of the environment. By addressing the relationships between building facades and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks, through an integrated package of requirements for street and building design, massing and scale

and setbacks, the Standards help in creating a unique character for the Specific Plan Area.

The Duarte Station Specific Plan is implemented through policies in this Specific Plan and Development Standards listed in this Specific Plan. The block pattern and the circulation framework is suggestive and the street locations, maybe be adjusted, though it is recommended that the future block and street pattern respect and connect to the existing surrounding context. For example, a street may be replaced by a publicly accessible pedestrian mews or an alley if it better serves the final development program, as long as it meets the intent of the Specific Plan.

### 4.1 GENERAL PROVISIONS

#### 4.1.1 Applicability

Upon adoption by Ordinance, this Specific Plan will constitute the zoning for the Duarte Station Specific Plan area. Subsequent Development Plans or agreements, tract or parcel maps, site plans, or any other action requiring ministerial or discretionary approval relative to the Specific Plan area must be consistent with the Development Regulations contained within this Chapter.

#### 4.1.2 Severability

In the event that any regulation, condition, program, portion or policy of this Specific Plan or the application thereof to any person or circumstance is held to be invalid or

unconstitutional by any court of competent jurisdiction, such portions shall be deemed separate, distinct and independent provisions and shall not affect the validity of the remaining provisions of this Specific Plan or applications thereof which can be implemented without the invalid provision.

#### 4.1.3 Determination of Unlisted Uses

Identification of any land use not specifically covered by the provisions contained herein shall be per the Duarte Development Code (DDC).

#### 4.1.4 Definitions

Unless otherwise specified below, terms used in this document shall have the same definitions provided in the City of DDC Chapter 19.160, "Definitions."

#### 4.1.5 Interpretation

Any ambiguities related to the implementation of this Specific Plan shall be determined as described in the DDC. Such interpretations shall take into account the stated goals and intent of this Specific Plan. Any interpretation made by the Community Development Director or designee may be appealed to the Planning Commission.

#### 4.1.6 Existing Users

Sites within the Specific Plan area that contain uses that are not otherwise consistent with the Specific Plan are deemed

"Legacy Sites," and the otherwise non-conforming uses on Legacy Sites are deemed "Legacy Uses." Legacy Uses on Legacy Sites shall be permitted to continue until such time as they are abandoned. A Legacy Use on a Legacy Site is deemed abandoned when the use has been discontinued for a continuous period of one year or more, and/or when the owner of a Legacy Site affirmatively indicates in writing that it has abandoned a Legacy Use.

At any time prior to abandonment, Legacy Uses on Legacy Sites may be expanded by an amount not to exceed five percent (5%) of the building square footage of the subject Legacy Site that existed as of the date of the adoption of the Specific Plan. If a Legacy Site is damaged or destroyed due to circumstances beyond the owner and the tenant's control, the Legacy Use on the Legacy Site may be reconstructed and/or repaired, and the Legacy Use may be continued/restored. If a Legacy Site is expanded, reconstructed or repaired in accordance with this paragraph, it shall be subject to site plan and design review as outlined in Section 6 of this Specific Plan, but shall be subject to the development standards set forth for properties in the Industrial Zone, as specified in the Duarte Development Code.

It is the intent of this Specific Plan that Legacy Uses on Legacy Sites are allowed until such time as market conditions cause the property owners to wish to redevelop.

## 4.2 BUILDING USE REGULATIONS

The Duarte Station Specific Plan provides for the development of a mixture of land uses within the Specific Plan area; including residential, commercial, office, and hotel uses. For the purposes of this Specific Plan, all permitted and conditionally permitted uses have been classified into use types. Each use type is defined below and includes uses such as those listed in the applicable tables. The tables include specific permitted, conditional, and prohibited uses.

Uses listed as permitted are further regulated herein and are defined as those uses permitted by right subject to approval of site plan/design review. Uses listed as conditional are defined as those which require special consideration either for their impacts on the neighborhood or their design.

Those uses expressly permitted within the Specific Plan area are included in Table 4-1, *Allowable Uses*. The use table is to provide a broad range of allowable uses which promote a compact, urban transit oriented development. Additional uses may be allowed in the future provided they meet the intent and the vision of the Specific Plan. Uses not listed shall be determined as described in the DDC

**Table 4-1  
Allowable Uses**

Uses or Activity	High Density Residential	Mixed Use	Plaza Mixed Use
<b>Residential</b>			
Multifamily dwelling units such as stacked flats, apartments, condominiums, with subterranean or structured parking, with a minimum density of 40 du/ac and maximum density of 70 du/ac	P	P	--
Single Family/Multifamily dwelling units, less than 40 du/ac within a parcel where the parcel average is at least 40 du/ac	P	P	--
Live/work units where the flex space does not require commercial mechanical or plumbing systems	P	P	--
<b>Retail (less than 12,000 sf)</b>			
Convenience uses such as small scale food sales, delicatessens, cafes, bakeries, florists, and general retail uses	--	P	P
Vendor carts	--	C	C
Vehicle Rentals, including car sharing	--	C	C
Retail centers more than 12,000 sf, or regional serving retail	--	--	--
Eating and Drinking Establishments, cafes	--	C	P
<b>Business, Financial, and Professional</b>			
Offices — Business or Corporate, Financial Institutions and Related Services	--	P	P

Uses or Activity	High Density Residential	Mixed Use	Plaza Mixed Use
Medical Services including medical and dental offices, physical therapy, medical laboratories	--	C	--
Research and Development (not associated with primary manufacturing), including research laboratories per DDC	--	P	--
Banks not including check cashing or personal lending	--	P	P
<b>Service Uses — General</b>			
Personal services such as barber shops, hair and nail salons, yoga studios, dry cleaning establishments.	--	P	P
Professional services such as mailing, duplicating and printing	--	P	P
Studio — Art, Dance, Martial Arts, Music	--	C	C
<b>Transportation, Communication, and Infrastructure Uses</b>			
Public and Private Parking Lots and Structures (not associated with a primary use)	--	C	--
Utilities, including Wireless antennae	C	C	C
<b>Service Uses — Restricted</b>			
Lodging, including hotels, motels, and extended stay facilities	--	P	--
Day Care (less than 8 children) per state law	P	P	P
P	Permitted		
C	Conditional Use Permit required		
--	Not Permitted		

### 4.2.1 Development Standards

The Duarte Station Specific Plan provides a framework to guide the development of a unique opportunity to transform the Plan Area into a vibrant, transit oriented development, key to which are the specific standards and guidelines tailored specifically to that vision.

The District Standards establish the Development Standards for all districts to guide future development and accommodate a variety of uses. While the Duarte Station Specific Plan defines setbacks for the Plan Areas, Building and Fire Codes in effect at the time of building permit shall take precedence.

Table 4-2 outlines development standards for each zoning district as defined by Figure 2-1, *Master Land Use Plan*. To provide flexibility, a large portion of the site is designated as Mixed Use, so for the illustration of standards the Development Scenario, (which illustrates one potential development scenario within the framework of the Master Land Use Plan) is used.

In addition to the specific Development Standards, design guidelines are also included. Where the Specific Plan is silent, the DDC and Municipal Code will prevail.

**Table 4-2  
Development Standards**

Land Use	High Density Residential	Office Mixed Use	Station Plaza Mixed Use
<b>Density/FAR (du/ac)</b>			
	Max.Density: 70 Min. Density: 40	Max. Density: 70 Min. Density: 40 Maximum FAR: 2.0	Maximum FAR: 1.0
<b>Block Dimension</b>			
Maximum 450' between publicly accessible paths of travel (vehicular or pedestrian) <sup>1</sup>			
<b>Maximum Building Heights (see Figure 4-1 for transitional requirements)</b>			
	65' or 6 stories <sup>3</sup>	90' or 7 stories for office or residential <sup>3</sup> 90' or 8 stories for Hotel <sup>3</sup>	45' or four stories <sup>3</sup>
<b>Minimum Ground Floor Height</b>	--	12' minimum clear (floor to ceiling height)	12' minimum clear (floor to ceiling height)
<b>Parapets</b>	Parapets may extend up to 3' above the height limit		
<b>Setbacks (see Figure 4-2 for setback requirements)</b>			
<b>Encroachments</b>	Porches, stairs, balconies, bay windows, awnings, may encroach up to 5' into required setbacks		
	Trash enclosures may be located in the setback as long as they meet requirements in the design guidelines		
<b>Parking Requirements</b>			
<b>Off Street Parking Standards</b>	Parking determined on a case by case basis during the site plan review	Per the City of Duarte Parking Standards	No dedicated parking required if on-street parking is available on streets fronting the plaza
<b>Access and curb cuts</b>	Maximum two curb cuts per block. Exceptions may be allowed during the project approval process		
<b>Tandem Parking</b>	Allowed if both spaces are shared by the same unit <sup>2</sup>	Not permitted	Not permitted
<b>Bike parking</b>	Secure storage	Residential: Secure	10 spaces

Land Use	High Density Residential	Office Mixed Use	Station Plaza Mixed Use
	area at 1 space per 4 units, exempting units with private garages	Storage area at space 1 per 4 units, exempting those with private garages Non Residential: 2.5 percent of the Parking requirement.	
<b>Group Usable Open Space</b>	200 sf per unit. Private open space <sup>4</sup> is not required for each unit. However, if provided, it may be deducted from the group open space requirement. Each square foot of private open space shall be considered equivalent to two square feet of group open space and may be so substituted.	Residential: 200 sf per unit. Private open space <sup>4</sup> is not required for each unit. However, if provided, it may be deducted from the group open space requirement. Each square foot of private open space shall be considered equivalent to two square feet of group open space and may be so substituted.	--
<p>Notes:</p> <ol style="list-style-type: none"> <li>1. Maximum dimensions do not apply on parcels adjacent to the rail line.</li> <li>2. Maximum allowable number of tandem parking spaces to be determined during the project approval process.</li> <li>3. Building height shall be measured from the adjacent finished ground level to the top of plate of the upper most story.</li> <li>4. Group usable open space shall have a minimum area of 300 square feet and a rectangle inscribed within it shall have no dimension less than 15 feet. Required usable open space may be located on the roof of an attached garage or carport. Private usable open space located at ground level shall have a minimum area of 100 square feet and a rectangle inscribed within it shall have no dimension less than 8 feet. The minimum area of above ground-level space shall be 50 square feet and a rectangle inscribed within it shall have no dimension less than five feet. Private usable open space shall be adjacent to, and not more than four feet above or below the floor level of the dwelling unit served. For projects that do not meet the Group Open Space standards in-lieu fees and other mitigation measures may be considered on a project by project basis</li> </ol>			

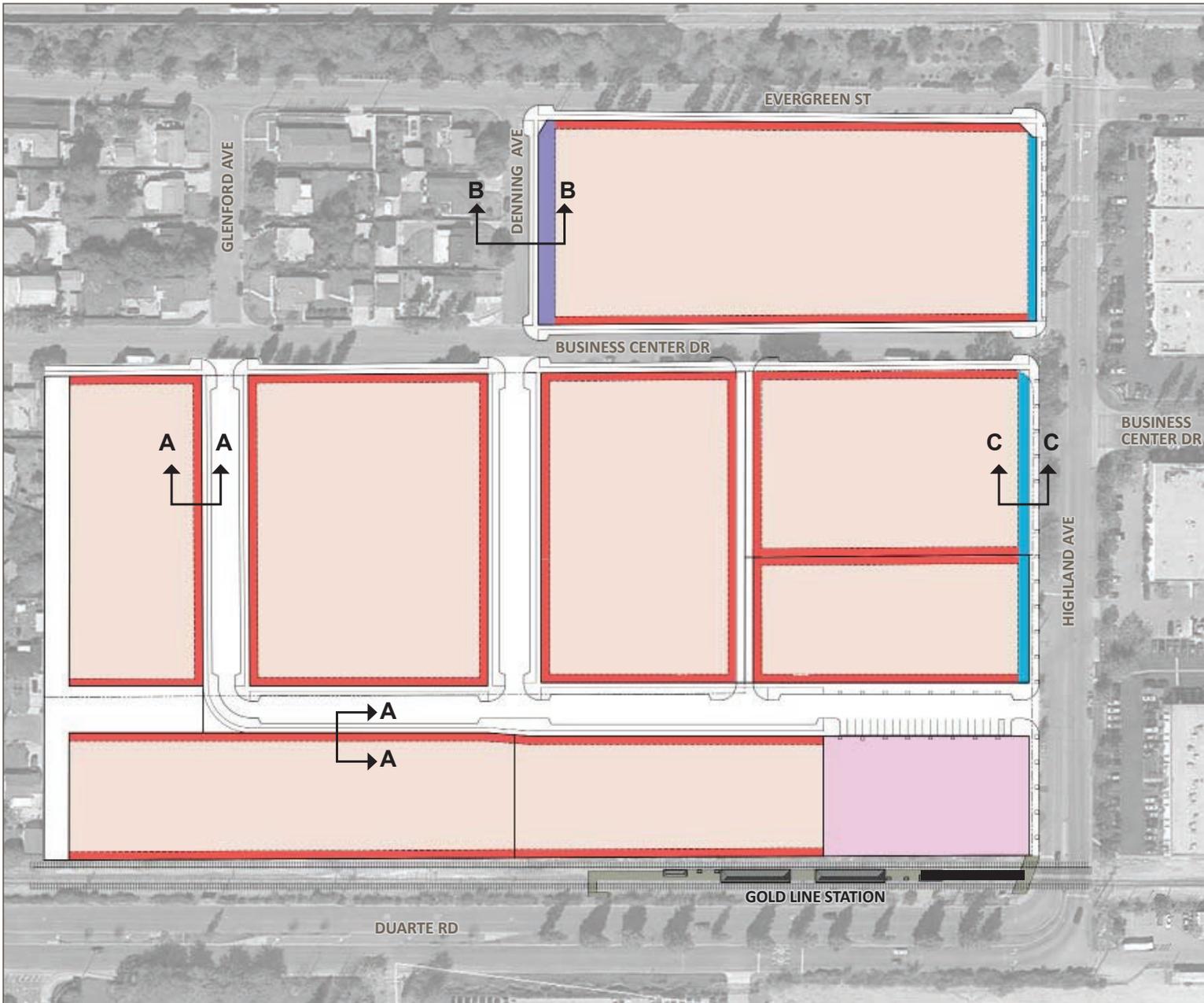
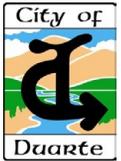
## 4.2.2 Building Setbacks

Building setbacks, along with the building edge, help define the character of the public realm street, especially at the pedestrian level. The setbacks are designed to take into consideration the streets that the setbacks are adjacent to, the intensity of proposed land uses, proposed building mass and scale, and the surrounding context and edge conditions.

The setbacks are measured from the back of the walk, right-of-way, or the property line, whichever is applicable, unless otherwise noted. Figure 4-1, *Building Setbacks* indicates the required setbacks within the Specific Plan Area.

Figure 4-2, *Building Setback Zones* illustrates specific setback sections A-A, B-B, and C-C defined in the plan.

- Section A-A represents a 10-foot setback zone applicable to most of the planning areas.
- Section B-B represents a 20-foot setback zone for the residential edge at Denning Avenue.
- Section C-C represents a 25-foot setback zone on Highland Avenue.



**REQUIRED BUILDING SETBACKS :**

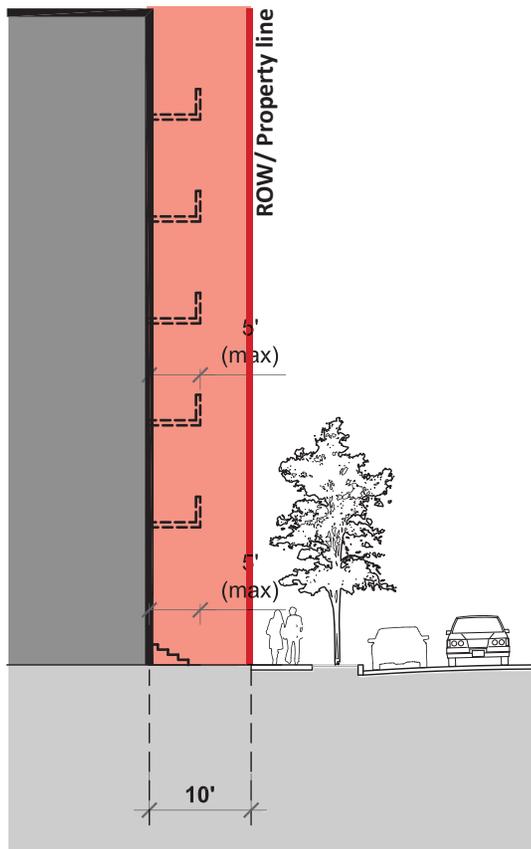
- 0' BUILDING SETBACK
- 10' BUILDING SETBACK
- 20' BUILDING SETBACK
- 25' BUILDING SETBACK\*

\* SETBACK MEASURED FROM FACE OF CURB OF HIGHLAND AVENUE

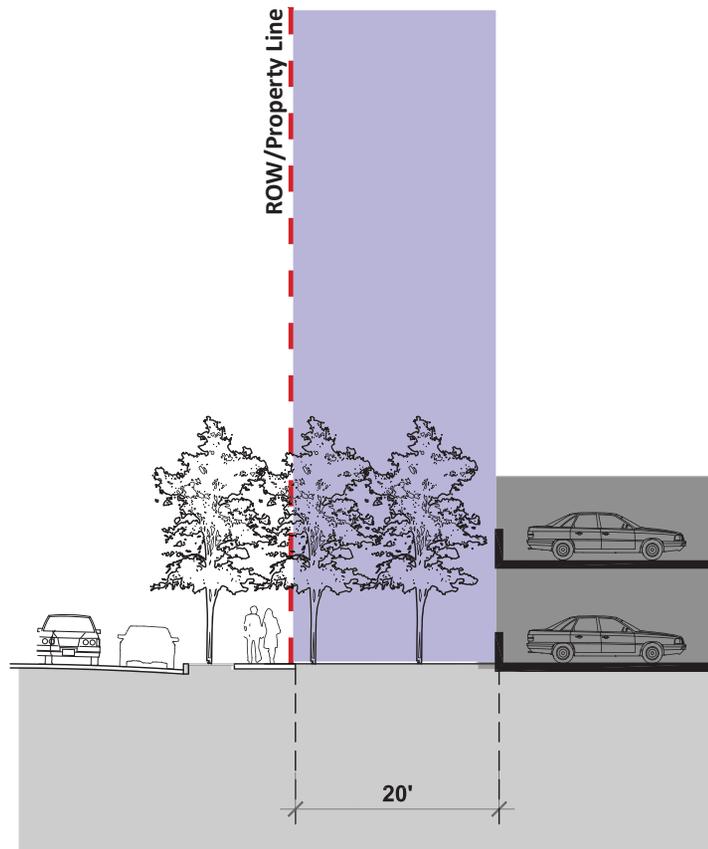
Source; DAHLIN group, 8-13



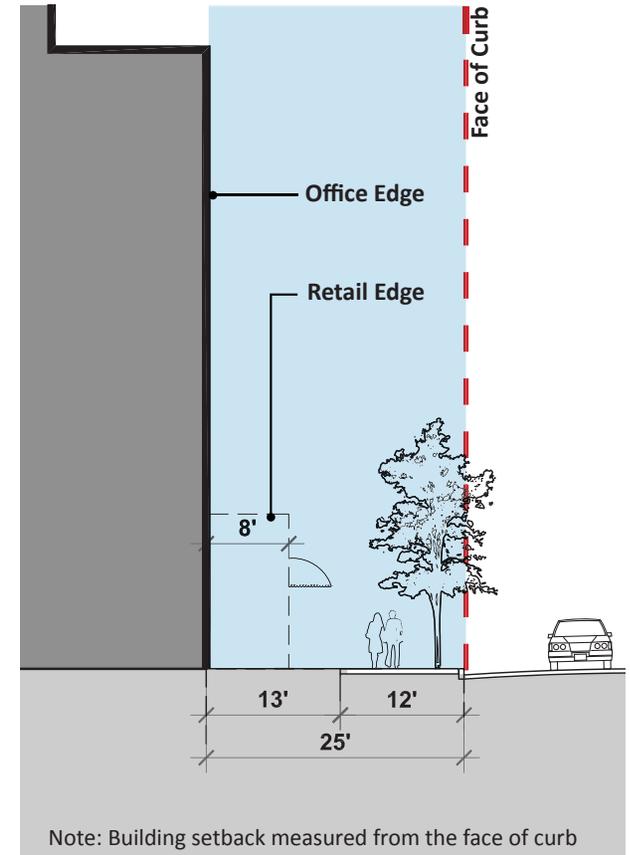
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**SECTION A-A: 10' BUILDING SETBACK**



**SECTION B-B: 20' BUILDING SETBACK**



**SECTION C-C: 25' BUILDING SETBACK**

Note: Building setback measured from the face of curb

Source: DAHLIN group, 8-13

### 4.2.3 Building Heights

Building heights help define the character of the public realm along the street, especially at the pedestrian level. The building height strategy is schematically shown in Figure 4-3, *Building Heights*. Building heights are designated to take into consideration the proposed intensity and type of development and the surrounding context and edge conditions. Building height shall be measured from the adjacent finished ground level to the top of plate of the uppermost floor. To accommodate flexibility in the pattern of development, the maximum building height is regulated by both the number of floors permitted and by total height. Number of floors shall include all habitable floors located above the average finished grade, and shall not include portions of the building substantially submerged or partly submerged below grade. This would accommodate a variety of land uses within the Plan Area while still controlling the urban form and character.

Portions of the building that extend above the primary building mass, such as dormers, roof-top cupolas, roof deck trellises, gazebos, and other special features, shall not exceed the maximum height requirement by more than 10 feet. Elevator and mechanical equipment are not subject to height limits as long they meet the screening design guidelines.

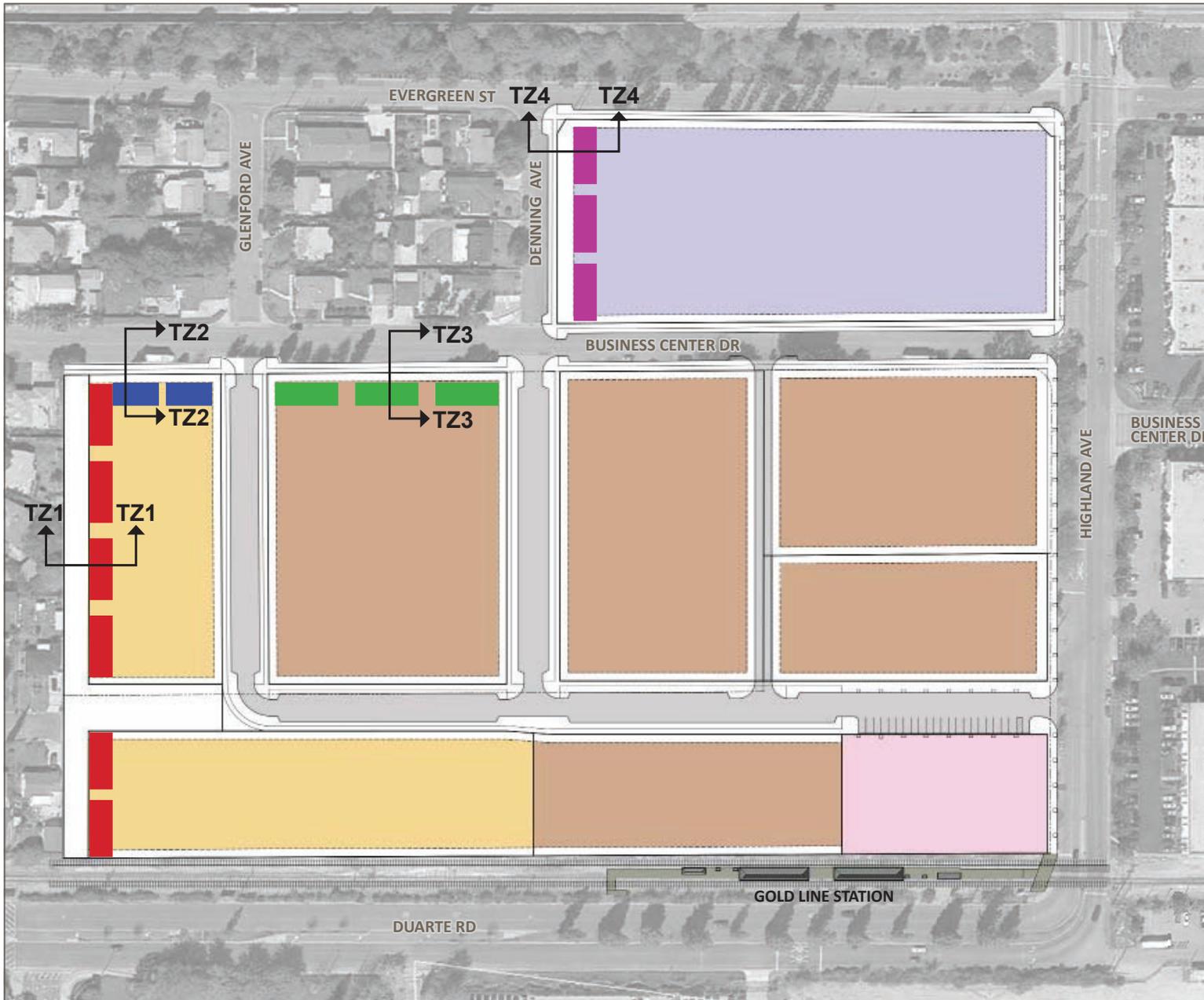
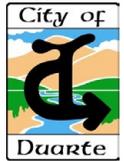
Where the proposed development area is adjacent to existing single family residential specific height limits and step-back conditions, referred in Figure 4-3, *Building Heights* as Transition Zones, are mandated. Figures 4-4 and 4-5,

*Residential Transition Zones*, illustrate the edge conditions and associated transition requirements.

### 4.2.4 Ground Floor Design

The design of the ground floor is of utmost importance in the Specific Plan area, in order to provide an attractive, comfortable, and safe environment for pedestrians. Good design establishes an attractive image and character for the area that makes it desirable for businesses and residents. A number of standards are established to ensure that goals for pedestrian scale are achieved.

- Buildings must face the street, and primary building entrances must be oriented towards the street.
- The facades facing major streets shall not have blank walls, service entrances, or other features that make the façade look like the back side of a building.
- Special standards and guidelines are established for the design of buildings with ground floor commercial space (see Table 4-2). Minimum floor to ceiling heights ensure that the space will serve the needs of retail and restaurant uses that may locate in the space at some point during the lifetime of the building.
- Floor elevations of buildings need to be at the sidewalk level, and the use of awnings, change in material, and architectural articulation should be used to create a pedestrian scaled public realm, despite the overall scale and mass of the building.



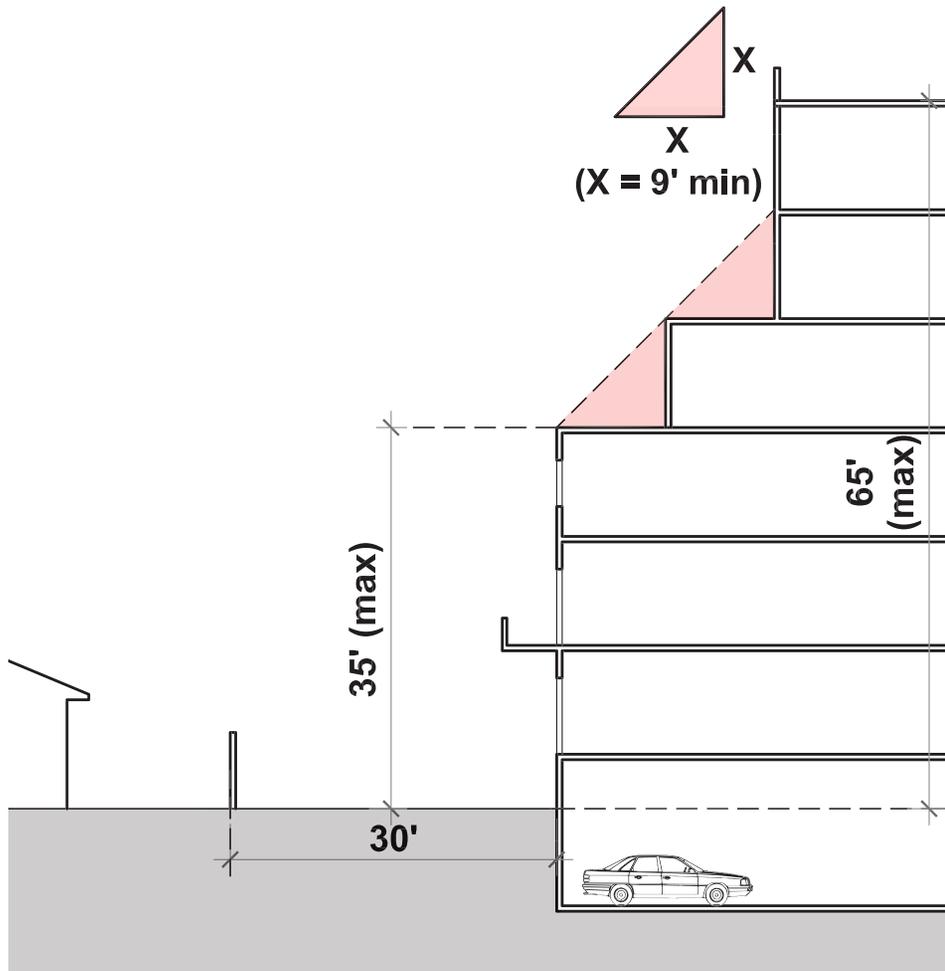
**MAXIMUM BUILDING HEIGHTS :**

- 4 STORIES OR 45'
- 6 STORIES OR 65'
- 7 STORIES OR 90'
- 8 STORIES OR 90'
- TRANSITION ZONE 1
- TRANSITION ZONE 2
- TRANSITION ZONE 3
- TRANSITION ZONE 4

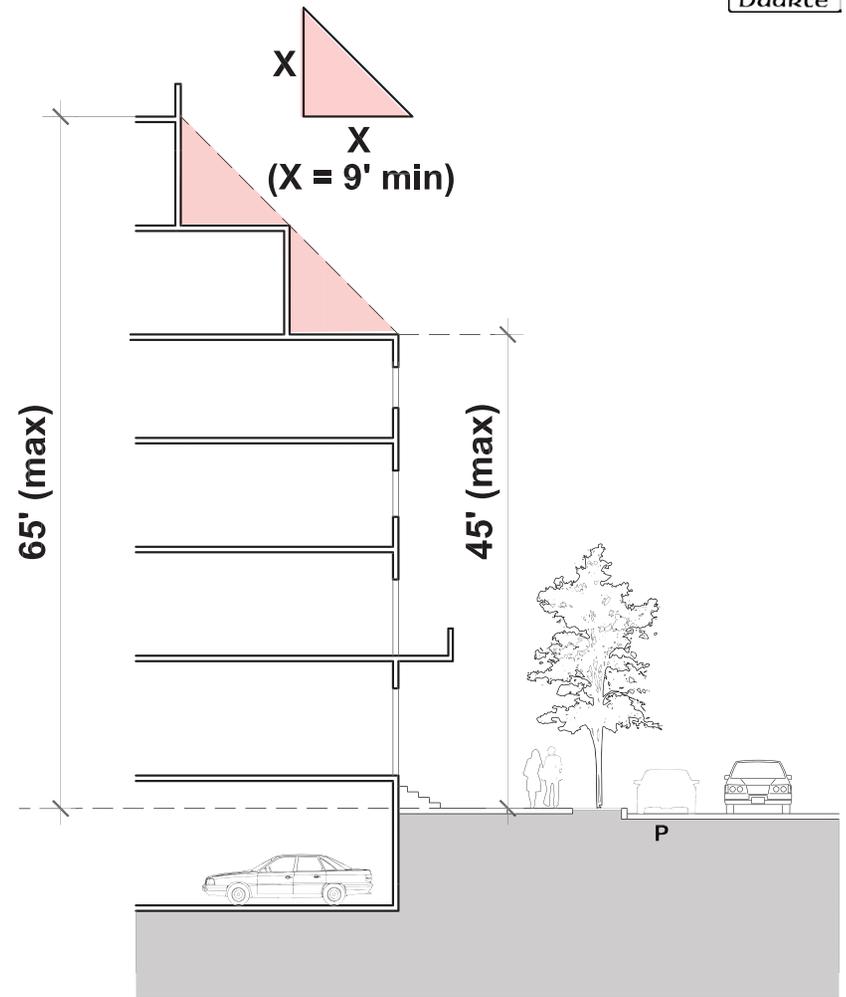
Source; DAHLIN group, 8-13



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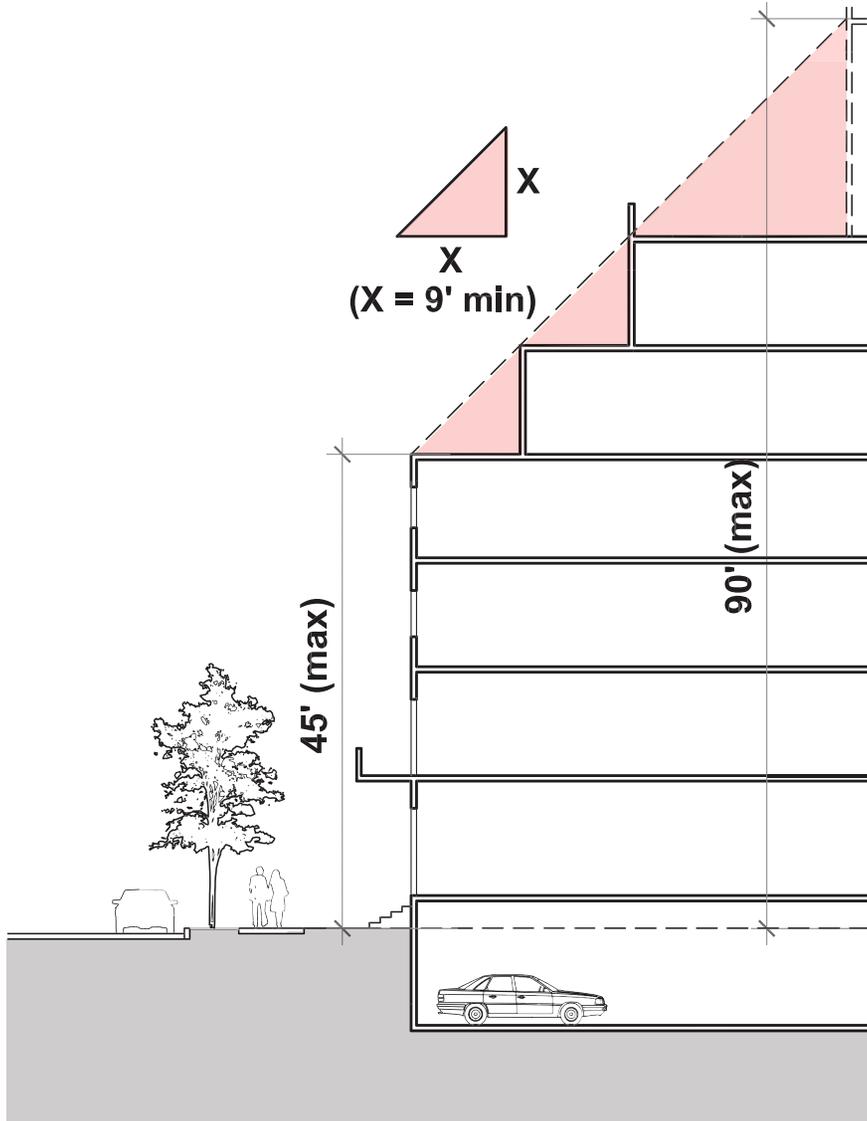


SECTION TZ1-TZ1: RESIDENTIAL TRANSITION ZONE 1

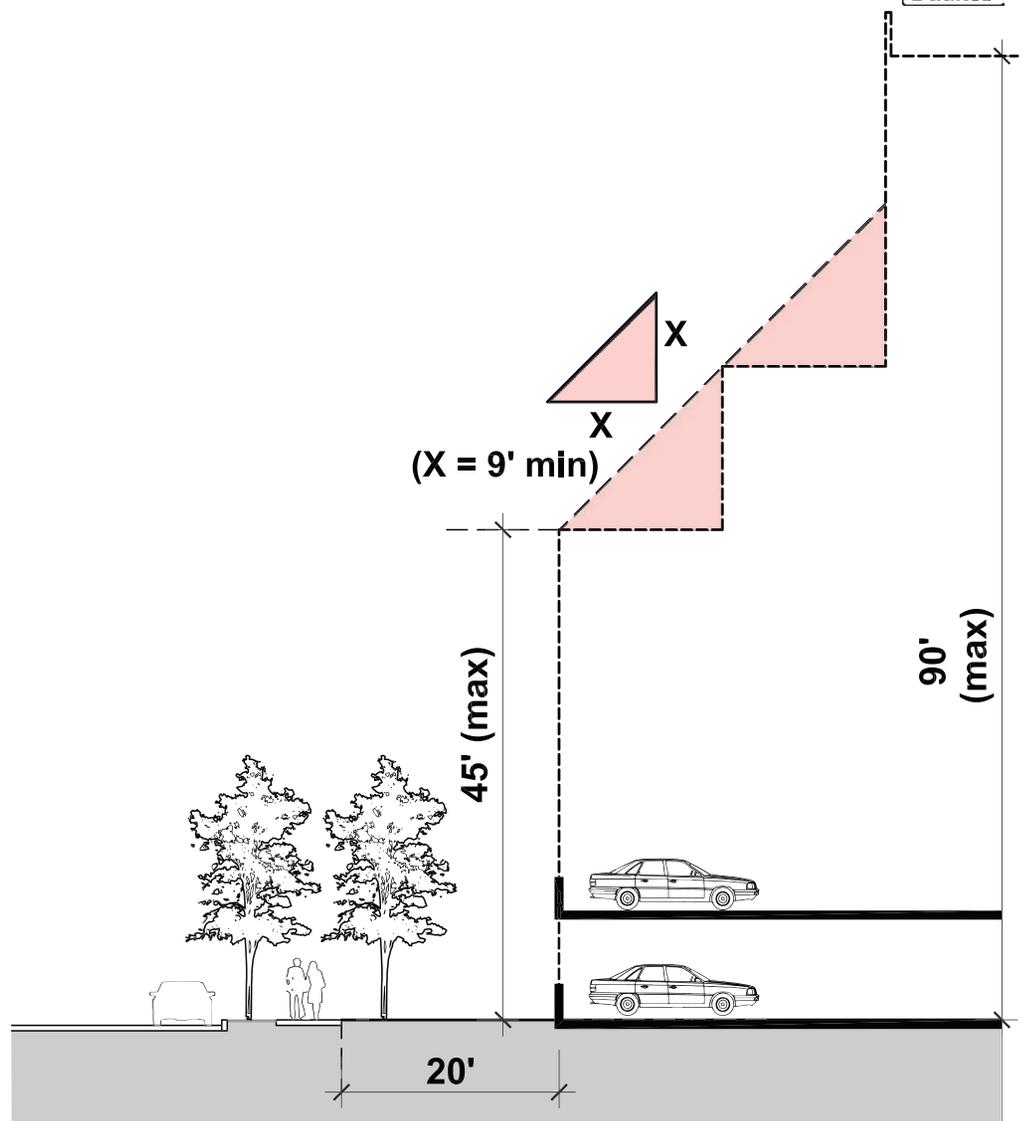


SECTION TZ2-TZ2: RESIDENTIAL TRANSITION ZONE 2

Source: DAHLIN group, 8-13



SECTION TZ3-TZ3: RESIDENTIAL TRANSITION ZONE 3



SECTION TZ4-TZ4: HOTEL/MIXED-USE TRANSITION ZONE 4

Source: DAHLIN group, 8-13

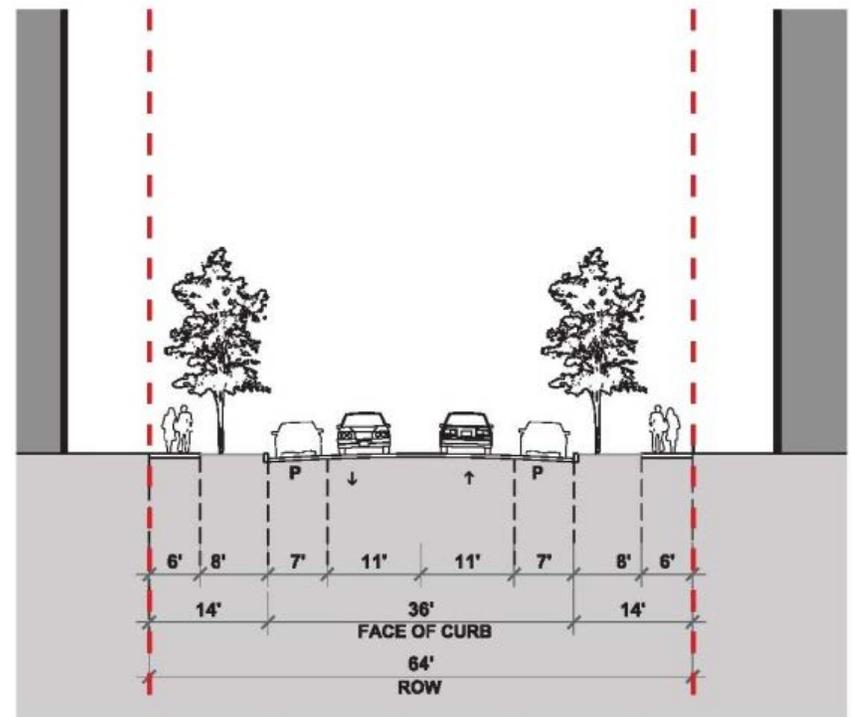
### 4.2.5 Street Standards

This section outlines the design requirements for existing and new streets within the Specific Plan Area. The street design standards are tailored to the type of street, the land use, and the building massing established in the overall plan. Since additional streets may be required beyond those currently indicated in the master land use plan, the Specific Plan also provides for standards and guidelines for future streets that may be needed as the area develops. Figure 4-6, *Street Section Index Map*, shows the major designated street types within the Specific Plan Area.

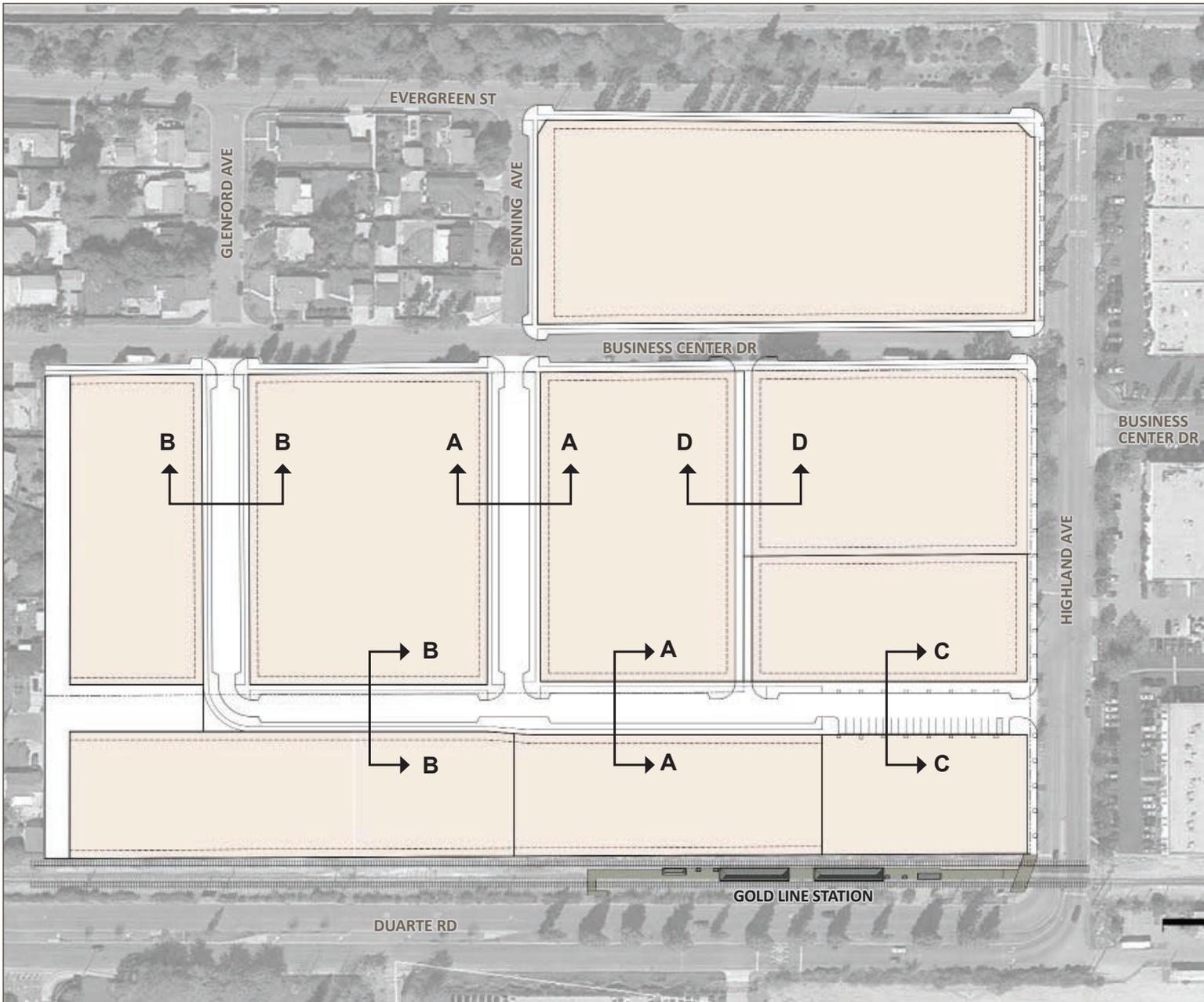
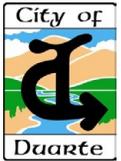
Detailed street sections of these streets and additional streets are provided in the following sections. The street section drawings specify the following street design standards:

- Travel Lanes Number and Dimensions
- Parking Lanes and Dimensions
- Planter Strips separating curbs and sidewalks
- Sidewalks Location and Dimensions

The 64' right-of-way local street (Section A-A on the Figure 4-6) will be the primary street within the Specific Plan Area. It will be the main vehicular and pedestrian connection from Highland Avenue and Business Center Drive to the Station Plaza. The street is designed to accommodate vehicular traffic as well as comfortable pedestrian circulation.



Section A-A 64-Foot Right-of-way Local Street



Source: DAHLIN group, 8-13

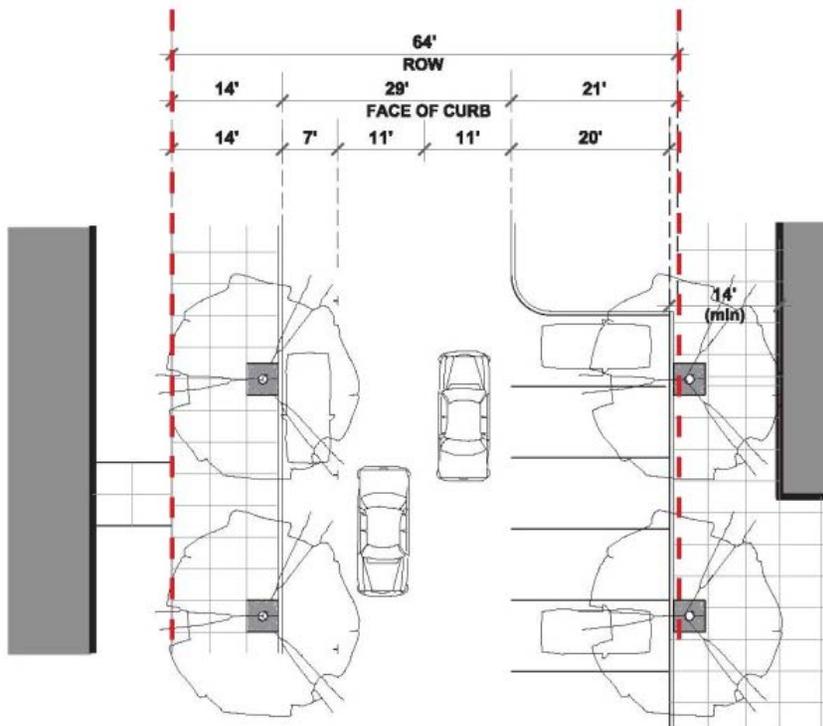


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### DUARTE STATION SPECIFIC PLAN Street Section Index Map

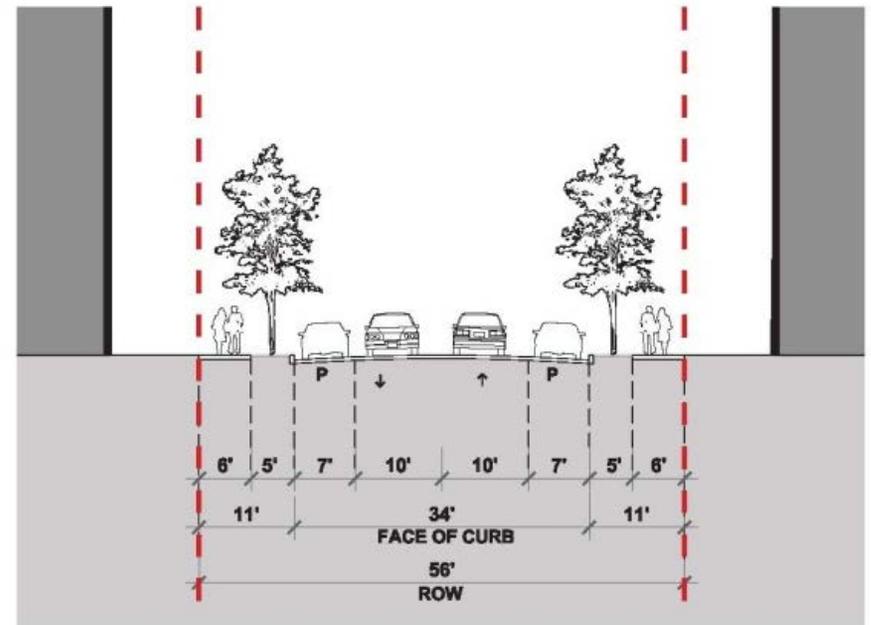
## Duarte Station Specific Plan

The 64' right-of-way street section is modified at the Station Plaza to create a more urban and pedestrian friendly character as shown below in Section C-C. The parallel spaces are converted to perpendicular spaces to accommodate for parking for the retail uses around the plaza. The trees are located in grates on the plaza and the plaza paving extends to the edge of the curb to form a continuous urban plaza.



Section C-C 64-Foot Right-of-Way Local Street at Station Plaza

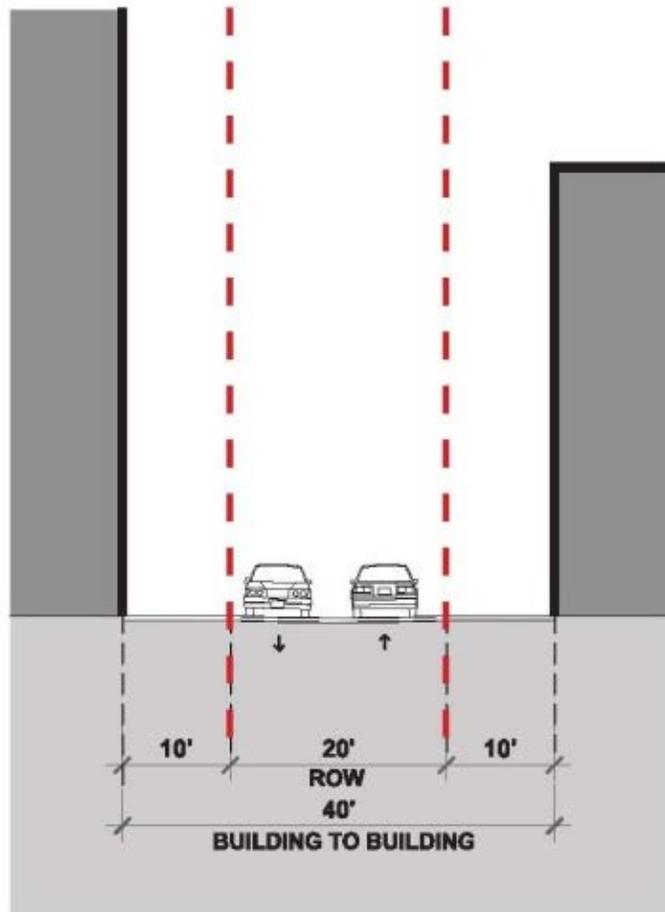
The 56' right-of-way street, as shown below in Section B-B, will be the local street within the Specific Plan Area. It will connect the proposed neighborhoods to the 64' right-of-way street or the surrounding existing streets. The street is designed for slow vehicular traffic and a comfortable pedestrian circulation.



Section B-B 56-Foot Right-of-Way Local Street

### 4.2.6 Typical Alley Section

The typical alley will be used to access service entries and parking structures. The alleys are designed for vehicular traffic and are not designed to accommodate pedestrian traffic.



Section D-D Typical Alley